

Speed Hump Policy

Transportation Department



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Traffic Calming 101

Common Requests

- Speed Humps
- Speed Cushions
- Stop Signs
- Driver Speed Feedback Signs
- Striping

Speed Hump



Speed Cushions



History of Current Speed Hump Policy

December 1997 – Policy formally adopted, typically only properties adjacent to the street included in survey and along cul-de-sacs or looping streets intersecting the subject street

January 2000 – Policy revised to expand survey area and include three properties **or** a minimum of 300 feet on side streets

July 2012 – Transportation Advisory Board (TAB) rejected to reduce the level of neighborhood support to something less than 70% and to only include properties adjacent to the street

January 2013 – Policy revised to remove the "**or**" clause to ensure consistent application: properties within a minimum of 300 feet on side streets to be included in survey

June 2016 – TAB rejected request to expand neighborhood survey area to include everyone who potentially drives the subject street

Current Process

Speed Cushions

Step 1: Traffic calming request made

Step 2: Staff evaluation; Fire Department Approval

Step 3: Conduct Speed Study (speed & volume)

Step 4: Neighborhood survey conducted
If speed and volume criteria have been met
At least 70% of affected owners must approve

Step 5: Two week public comment period

Step 6: Transportation Advisory Board (TAB) Approval

Step 7: Installation

Speed Humps

Step 1: Traffic calming request made

Step 2: Staff evaluation; Fire Department Approval

Step 3: Conduct Speed Study (speed & volume)

Step 4: Neighborhood survey conducted
If speed and volume criteria have been met
At least 70% of affected owners must approve

Step 5: Installation

Comparison by City: Engineering Process

	Eligible Streets	Max Posted Speed	Consider Emergency Routes	Speed Criteria	Volume Threshold
Mesa	Residential & Collector	30 MPH	Fire	85th percentile > 8 MPH Posted	500 to 5000
Phoenix	X	X	X	✓	X
Tempe	—	—	X	✓	—
Scottsdale	—	X	—	X	—
Chandler	—	—	X	✓	—
Glendale	X	X	—	X	—
Gilbert	X	—	✓	X	—
Peoria	—	—	—	✓	✓

X

 More Restrictive

—

 Similar

✓

 Less Restrictive

Refer to Table 1 in Appendix B of the report for more information

Comparison by City: Public Process

	Initial Support	Required Approval Rate	Properties included	City Funding available	Wider Support	Appeal Process	Removal Process
Mesa	Requester	70%/100% within 50 feet of Speed Hump	Homes abutting & within 300' on connecting streets	Yes Citizen only pays \$58 for counts	Yes (TAB)	No	51% approval after 12 months
Phoenix	X	X	✓	X	✓	—	—
Tempe	—	X	—	—	✓	—	X
Scottsdale	X	—	—	X	—	X	X
Chandler	X	X	X	—	—	—	X
Glendale	—	✓	—	—	✓	—	X
Gilbert	X	X	X	X	✓	—	—
Peoria	—	—	✓	—	✓	—	X

X More Restrictive

— Similar

✓ Less Restrictive

Refer to Table 2 in Appendix B of the report for more information

Feedback

Speed Cushions

Step 1: Traffic calming request made

Step 2: Staff evaluation; Fire Department Approval

Step 3: Conduct Speed Study (speed & volume)

Step 4: Neighborhood survey conducted

*If speed and volume criteria have been met
At least **70% of affected owners** must approve*

Step 5: Public comment period

Step 6: Transportation Advisory Board (TAB) Approval

Potential Appeal Process

Installation

Speed Humps

Step 1: Traffic calming request made

Step 2: Staff evaluation; Fire Department Approval

Step 3: Conduct Speed Study (speed & volume)

Step 4: Neighborhood survey conducted

*If speed and volume criteria have been met
At least **70% of affected owners** must approve*

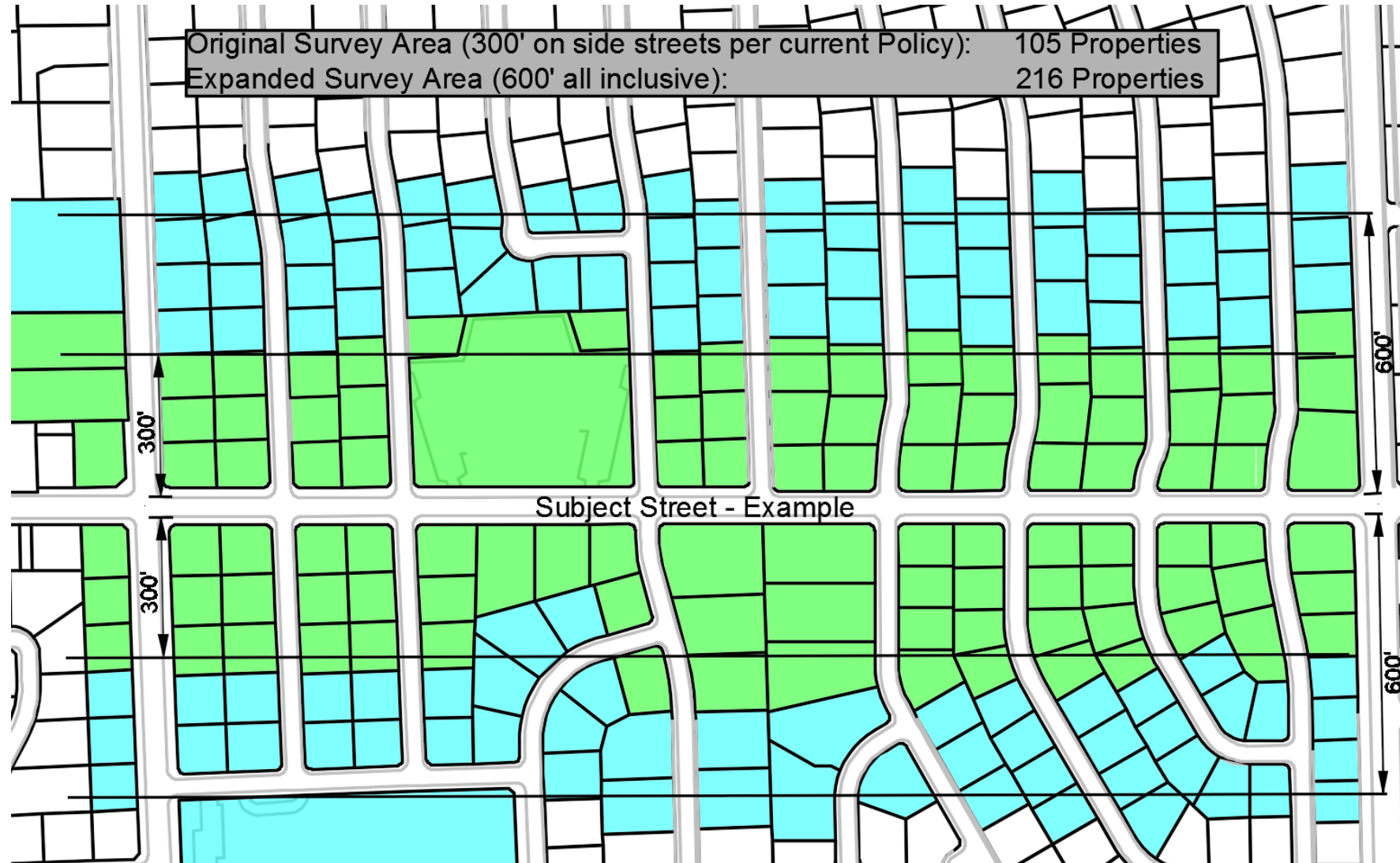
Public comment period

Transportation Advisory Board (TAB) Approval

Potential Appeal Process

Installation

Example: Neighborhood Survey Area Expansion



Policy Direction

Staff is seeking policy direction from SAT on:

- Properties included in neighborhood survey and approval rate
- Potential appeal process
- Public comment phase and TAB approval for speed humps

Discussion & Questions

