NARRATIVE REPORT FOR VILLAS ON MAIN

Located in Mesa, Arizona.

Application for Rezoning & Site Plan Approval

BFHGroup

Prepared By:

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1. APPLICATION REQUEST

BFH Group is delighted to present to the City of Mesa this application with corresponding narrative and exhibits for the Rezoning application and Design Review submittal packages. Per request by the applicant, the purpose of the narrative is to explicitly request rezoning of the property from Limited Commercial to Multiple Residence 3 with a U designator and a Planned Area Development Overlay (RM-3-U-PAD), as well as for a Site Plan Review to the proposed 5.56 acre townhouse residential development located at 6147 E. Main Street, Mesa, AZ 85205, more specifically APN: 141-57-081B. The proposed development is intended to be 86 residential units with a proposed gross density of 18.30 du/ac.

2. LOCATION AND DESCRIPTION OF SURROUNDING AREA

The property is located on the south side of East Main Street approximately 650 feet west of South Recker Road. It is currently vacant undeveloped land.

Immediately west of the proposed site is an existing empty commercial building that is owned and maintained by Schroeter Family Trust as well as an occupied office building owned and maintained by ALS West Inc. To the south are offices owned and maintained by Baywood Professional Plaza I Association. To the east of the site are existing retail shops and restaurants owned by various owners. Coordination efforts have already been initiated. To the north of the site are more retail shops and restaurants all associated with the Buckhorn Center.

The local retail, offices and restaurants are considered assets to this site, not to mention a residential development of this caliber would help spur commercial activity in this area. The residential buildings have been arranged around the perimeter of the site with a central drive aisle. The drive loops around the entirety of the site allowing required fire and trash access. This design also opens the entire site to pedestrian access to the adjacent commercial and offices. The proposed development boasts 2-story townhouse style units each with a private 2-car garage throughout the entire development. The townhouse units feature a 7' to 10' private rear yard/patio with a landscaped path to the perimeter wall. Required open space will be provided to every residential unit with a private patio, but well over an acre will be available to the community for open space. The townhomes will not be for sale individually, but for rent. It is not the intent of the developer to subdivide each unit.

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3. COMPLIANCE WITH GENERAL PLAN & ZONING

As already discussed, the property is surrounded on three sides by existing developments. All surrounding properties, including the subject parcel, fall under the City of Mesa General Plan designation as "Mixed Use Activity District". On the north, are found several existing multi-family residential subdivisions including Villas Tuscany and Dreamland Villas, which are currently RM-2 and R-4 (County) zoning, respectively. On the south and west, are found more existing multi-family residential developments that are all currently zoned RM-4. (See Figure 3 – General Plan Land Use; Figure 4 – Zoning Map)

The property is currently zoned Limited Commercial (LC) per the City of Mesa. Based on previous discussion with City staff and per the development standards for LC, it is the intention of the developer to propose rezoning from LC to RM-3-U and Site Plan Approval for this property to be developed as a multi-family residential use with 86 two-story townhome-style units. It is further understood by the applicant that the property has been vacant undeveloped land for well over three decades.

Based on review of the area, both the current zoning and General Plan Land Use, residential development is an agreeable use for this property. The location is ideal as it sits adjacent to existing shops and restaurants, central to several schools and high schools within the Mesa School District, close proximity to both the US 60 and 202, and the site provides a unique residential product that is much needed within the City and this area. The property has sat vacant for well over 30 years, aggregating trash, debris, transients, as well as nuisance vegetation. Development of this property will not only be a tremendous boost to the character of the area, but also will make the area safer and more appealing to current and future residents. The proposed development *Villas on Main* will continue to support the City of Mesa's goals, policies, development standards and strategies as set forth in the General Plan and Zoning by promoting a safe, superior and unique living experience.

It is important for the developer to note that there are several zoning ordinance standards that will be slightly deviated from through this zoning application. Below is a list of each standard, why the deviation is requested, and how the developer intends to compensate for the variation.

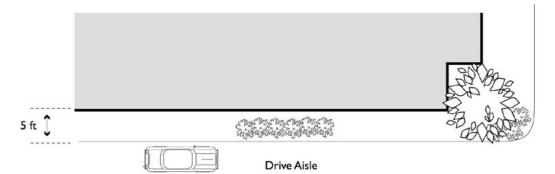
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PLANNED AREA DEVELOPMENT STANDARDS

RM-3-U (TRANSIT CORRIDOR)

CURRENT STANDARD	PROPOSED STANDARD
REDUCTION OF THE FOUNDATION BASE FROM	FOUNDATION BASE = 2' ALONG BUILDING
5' MINIMUM	GARAGE FRONTS; 5' MIN EVERYWHERE ELSE.
PRIVATE PATIO MIN. DIMENSION = 10 ft	PRIVATE PATIO MIN. DIMENSION = 5 ft
MIN 3' RECESSED GARAGES	ENHANCED GARAGE FASCIA

REDUCTION OF FOUNDATION BASE: Per City of Mesa Zoning Ordinance 11-33-5(A)(2)(b), A minimum 5-foot-wide foundation base shall be provided, measured from face of building to face of curb along the entire length of the exterior wall adjacent to drive aisles as illustrated.



It is proposed that along the frontage of the units that consist of garages, there will be a varying distance from the face of the building to face of curb. The minimum distance will be 2', instead of 5', but this would only affect the following quantity of units:

Two (2) ft min. 16 UNITS

Three (3) ft min. 38 UNITS

Five (5) ft min. 32 UNITS (compliant to standard)

The reduction to this standard is only requested for the portion of the buildings where the garage fronts onto the street and does not affect all the units. It is important to note that all other scenarios, such as the side of the building facing the street, the foundation base minimums are compliant to this standard. The reason for the request is due to the "alley loading" layout which is more cohesive to a fluid and ample open space plan focusing the community to "front yards" rather than the streets. Considering

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this, the developer requests deviation to this standard in order to provide a more robust and abundant open space to promote community interaction and superior living experience.

PRIVATE PATIO MINIMUM DIMENSION: The proposed units at *Villas on Main* are all 21 ft wide. If the developer were to propose a minimum dimension of 10 ft for the patio areas as shown, this would equate to 210 sf per unit, however the minimum required open space per unit is 150 sf, per MZO Table 11-5-5. The location that the developer is proposing the 5 ft patios is only along the perimeter east, south and west (only 22 units affected). It is important to mention that the dwelling units undulate throughout the site and as such, nearly one-third of the patios along the perimeters will be 6' in depth (12 units total). At all these locations, the residents are fortunate to enjoy an additional 15' beyond the patios that is essentially all their own with lush landscaping and a 5 ft meandering sidewalk that connects to adjacent amenities including pool, totlot, pickle ball court and/or ramadas. Residences along the interior of the site enjoy 8 ft patios that will boast a total of 168 sf of private open space, which is well above the minimum standard (32 units total). Residences along E. Main Street also propose a 7ft-8ft patio which is compliant with the dimension standard also providing 147-168 sf of private open space (20 units total). As proposed, the average private patio offers 145.5 sf/du, which is slightly below the standard minimum. Outside of the private patios, the overall site plan delivers an additional 69,773 sf of community open space. This equates to approximately 811.3 sf/du. In view of the compensation for the deviation, reducing the minimum standard is a mutually beneficial compromise and expands the overall open space presented to the community as a whole, thus offering a superior community experience.

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MINIMUM 3' RECESSED GARAGES: The proposed units at *Villas on Main* will have a proposed overhang that will give the appearance of a recessed garage, but it will not quite meet the letter of the ordinance as spelled out in Section 11-55-5(B)(6) of the MZO. With the proposed architectural overhang along the garage frontage, the developer intends to provide enhanced garage fascia that would give offer a distinct aesthetic with larger vertically hung windows to accent the variation between townhomes and promote sense of uniqueness within the community. Shown below.



The development meets the City of Mesa Zoning Ordinance Table 11-32-3.A, for required on site parking. This is achieved with 2-car garages and onsite parking areas. The total required parking for the site is 86 x 2.1 stalls/unit = 180.6 or 181 total stalls. The proposed parking will be as follows 86 x 2 stalls per unit + 14 open stalls = 186 total stalls.

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4. OPERATIONAL ASPECTS OF USE

East Main Street is intended to remain as is with minor improvements that may include sidewalk and landscaping along its frontage. No significant improvements along East Main Street are anticipated at this time.

The site is a proposed multi-family residential community. It is not expected to be gated at the entrance, and view fence along East Main Street shall be proposed. The balance of the perimeter will be a 6' masonry wall including the rear and sides.

The property will be managed by either onsite or offsite property management. Either the property management or an established HOA will maintain internal driveways, infrastructure, buildings, landscaping, amenities, etc. Typical hours of operation will be 9 am to 5 pm, but considering the nature of the property, it is understood that these hours are not absolute. It is anticipated that the development is intended for newly married / younger family and retiree residents.

5. PROPOSED INFRASTRUCTURE

Electric service is provided by SRP. Domestic Water and sewer will be provided by the City of Mesa. Refuse is by private contract. Police is provided by the City of Mesa. Fire is the City of Mesa. No natural gas is expected be used on this development. All utilities within the site will be privately operated and maintained except for water and sewer mainlines, which an easement will be provided on.

PUBLIC UTILITIES	SERVICE PROVIDERS
Fire Protection	Mesa
Police Protection	Mesa
School District	Mesa Unified School District
Water	Mesa
Wastewater	Mesa
Telephone	Century Link, Cox
Natural Gas	NA
Refuse	Private Contractor
Electric	SRP

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As noted, East Main Street is fully developed including meandering sidewalk along the property frontage. It is not expected that the developer will be required to make significant improvements to East Main Street except for tying into the existing water and sewer. As part of the improvements to the site, the existing drainage pattern will be slightly modified, however the developer and his engineer will ensure that all onsite retention requirements are met per the City of Mesa Engineering and Design Standards. At time of the Development Application, a full Preliminary Drainage Report shall be provided for approval.

6. DEVELOPMENT SCHEDULE

It is not expected that this property will be developed in phases. A development schedule has not been established as of yet, however it is expected that the permit and entitlement process at the City will take approximately 8-12 months for final approvals. At which time, the developer will initiate construction and construction is expected to take another 9-12 months depending on market conditions.

7. CONCLUSION

Villas on Main is ideal for this residential development. There is a demand in the area for affordable / attainable housing in Mesa and inadequate supply. More housing in this area will generate a higher demand for commerce, including supermarkets, convenience stores, offices, retail, etc. in the close vicinity of the proposed development. It is the intent and desire of the developer to work hand in hand with the City as well as the neighbors to create a viable development that benefits not only the existing neighborhood, but also the future residents.

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APPENDIX A

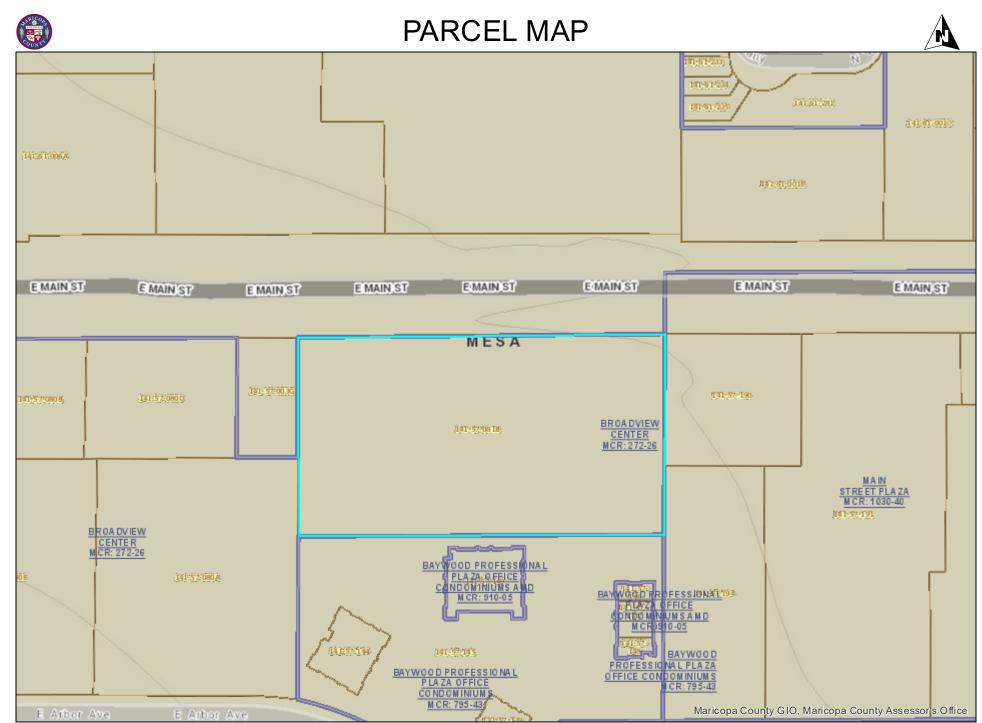
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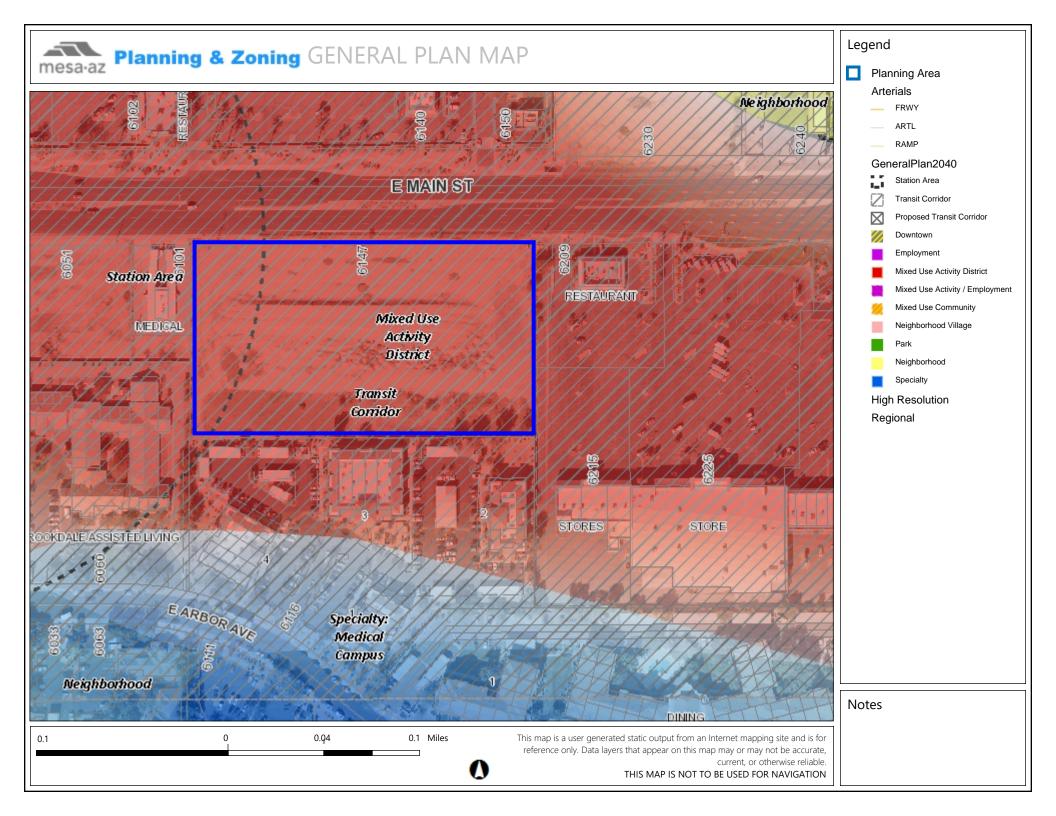
AERIAL MAP

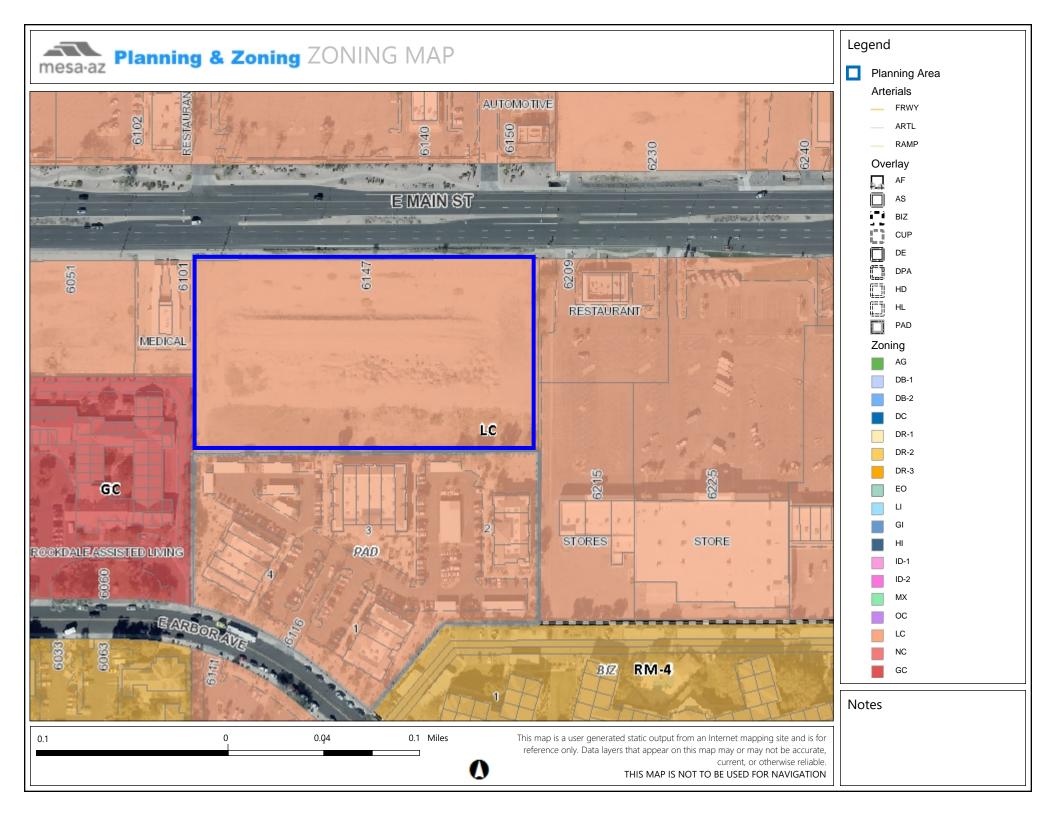


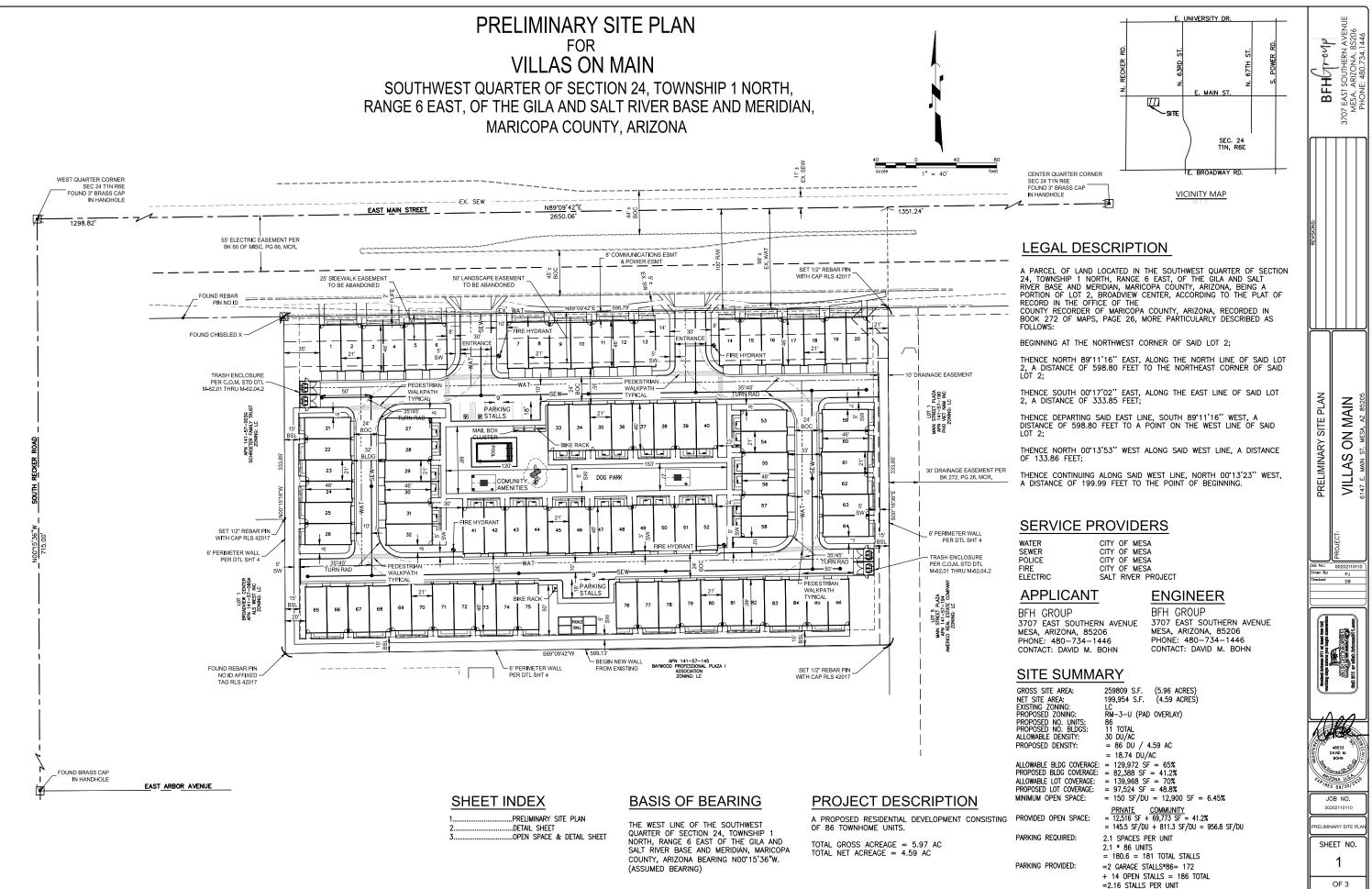
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SITE AREA:	259809 S.F. (5.96 ACRES) 199,954 S.F. (4.59 ACRES) LC
POSED ZONING:	RM-3-U (PAD OVERLAY)
Posed No. Units: Posed No. Bldgs:	86 11 TOTAL
WABLE DENSITY:	30 DU/AC
POSED DENSITY:	= 86 DU / 4.59 AC
	= 18.74 DU/AC
	= 129,972 SF = 65%
	= 82,388 SF = 41.2%
	= 139,968 SF = 70%
	= 97,524 SF = 48.8% = 150 SF/DU = 12,900 SF = 6.45%
NOW OF EN STACE.	•
/IDED OPEN SPACE:	$\frac{PRIVATE}{12,516} = \frac{COMMUNITY}{4000000000000000000000000000000000000$
	= 145.5 SF/DU + 811.3 SF/DU = 956.8 SF/DU
(ING REQUIRED:	2.1 SPACES PER UNIT
	2.1 * 86 UNITS
	= 180.6 = 181 TOTAL STALLS
(ING PROVIDED:	=2 GARAGE STALLS*86= 172
	+ 14 OPEN STALLS = 186 TOTAL
	=2.16 STALLS PER UNIT